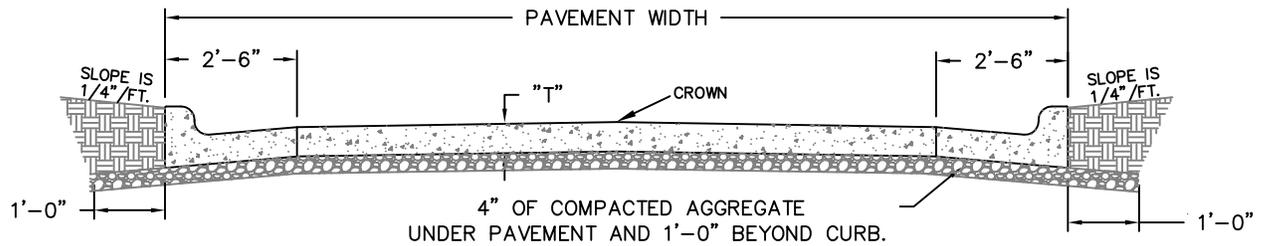
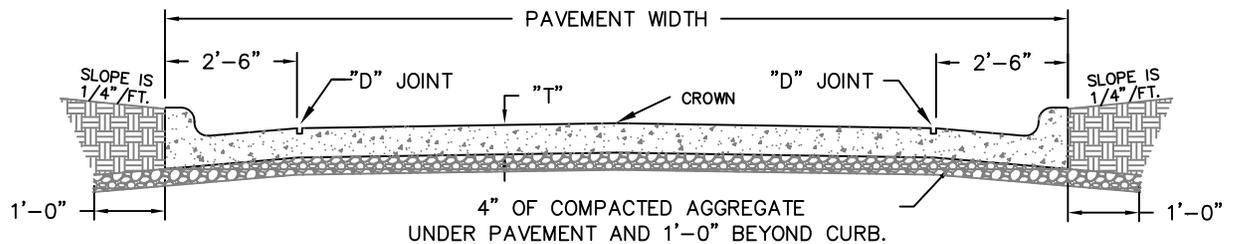


BITUMINOUS PAVEMENT WITH CONCRETE CURB & GUTTER



PORTLAND CEMENT CONCRETE PAVEMENT AND CONCRETE CURB & GUTTER



PORTLAND CEMENT CONCRETE PAVEMENT WITH INTEGRAL CURB

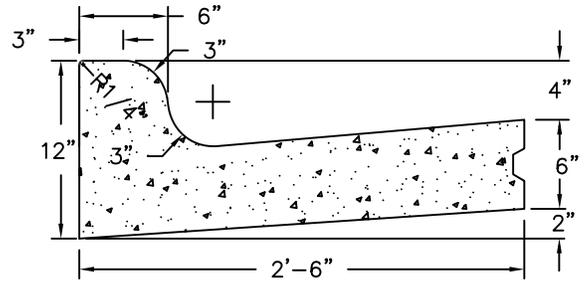
STANDARD PAVEMENT WIDTH AND THICKNESS			
STREET TYPE	"T"	"A"	"B"
ALLEY/LOCAL	6"	2"	5"
COLLECTOR	6"	2"	6"
SECONDARY ARTERIAL	7"	2"	8"
PRIMARY ARTERIAL	8"	2"	9"

NOTES: CROSS SLOPE SHALL BE 1/4"/FT. ON ALL PAVEMENTS EXCEPT ALLEYS, SEE STANDARD DRAWING ST-4.

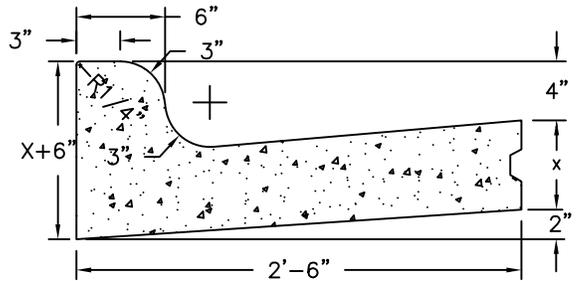
WIDTH OF PAVEMENT IS SUBJECT TO REQUIREMENTS OF THE PLANNING DEPARTMENT AND MAY VARY FROM THE STANDARDS.

TO CREATE A GUTTER SECTION THE CONTRACTOR SHALL FORM A 2" RISE 2' FROM THE INSIDE OF THE CURB. PAVEMENT WIDTH WILL BE MEASURED FROM BACK OF CURBS ON IMPROVED STREETS. PAVEMENT CROWN SHOULD BE CENTERED IN RIGHT OF WAY.

NOTE: KEYWAY OR #5 REBAR @ 2' 6" O.C. TO BE USED ON ALL CONCRETE PAVEMENT. ASPHALT PAVEMENT - DOWELL AND KEYWAY SHALL BE OMITTED

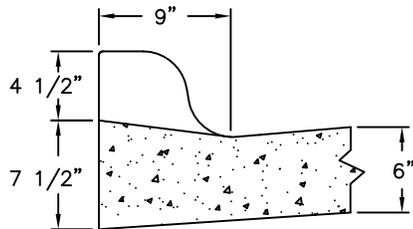
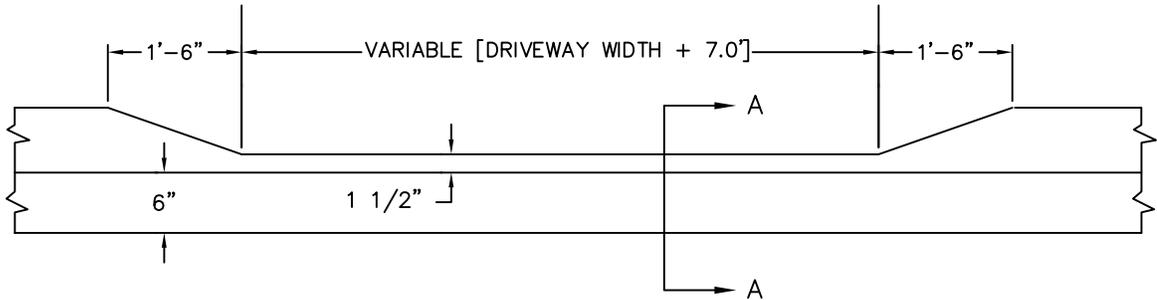


ALLEYS, LOCAL, AND COLLECTOR STREETS

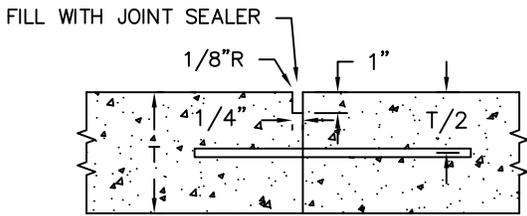


COLLECTOR, 2ND ARTERIAL & HIGHER
X TO MATCH PAVEMENT THICKNESS;
8" MAX FOR ASPHALT PAVEMENT

STANDARD RESIDENTIAL DRIVEWAY OPENING

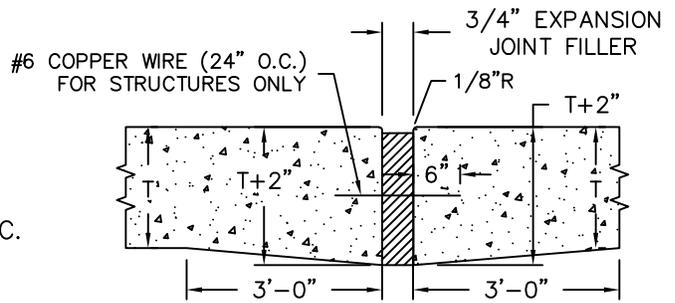


SECTION A-A

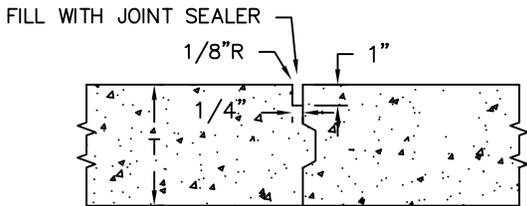


DOWELED LONGITUDINAL JOINT WITH 24" No.6 DEFORMED BAR AT 24" O.C.

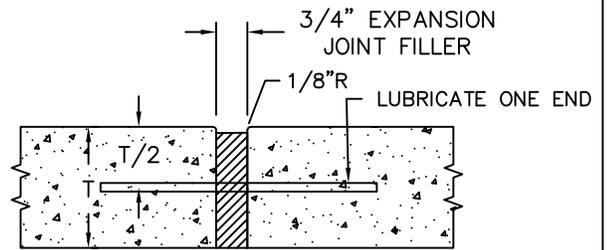
TYPE "A"



EXPANSION JOINT TYPE "E"

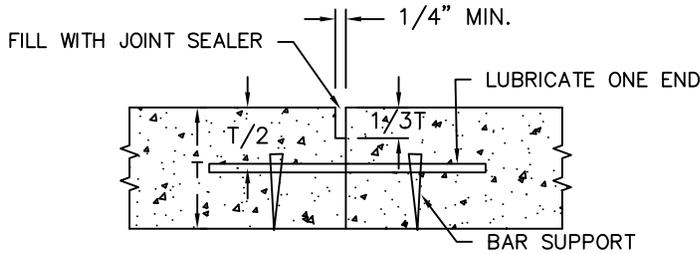


KEYED LONGITUDINAL JOINT TYPE "B"



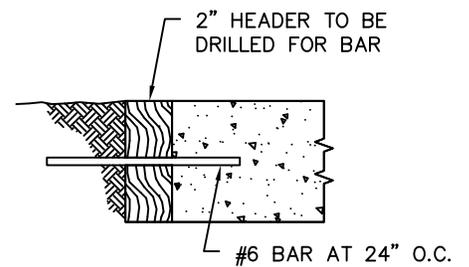
DOWELED EXPANSION JOINT 3/4" x 15" DOWEL BAR AT 24" O.C.

TYPE "F"

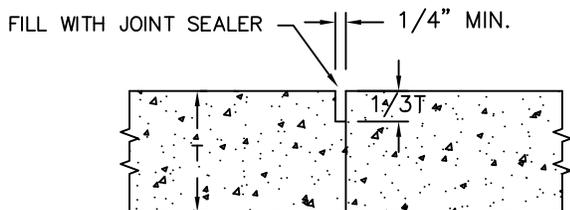


SAWED CONTRACTION JOINT WITH 1" x 18" DOWEL BAR AT 24" O.C.

TYPE "C"

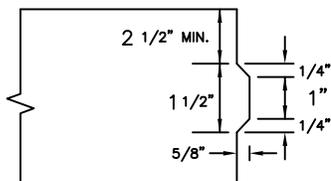


CONSTRUCTION HEADER CONCRETE



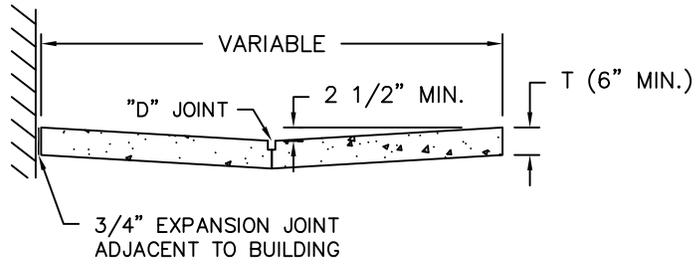
SAWED CONTRACTION JOINT TYPE "D"

DEPTH OF SAW JOINT: 1/3 OF PAVEMENT THICKNESS

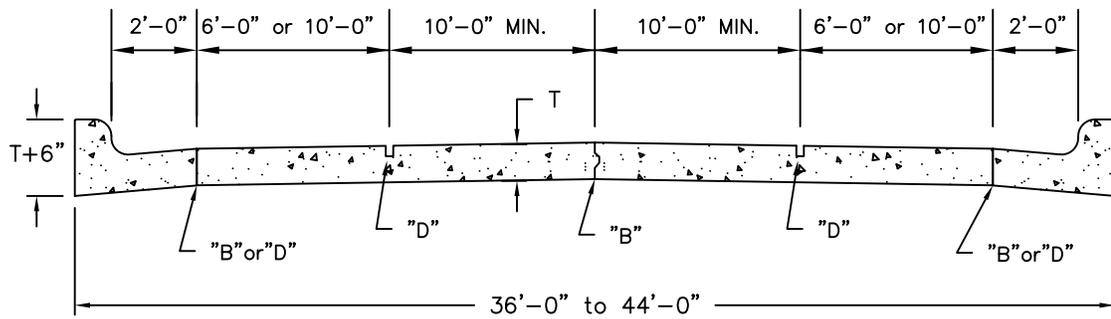
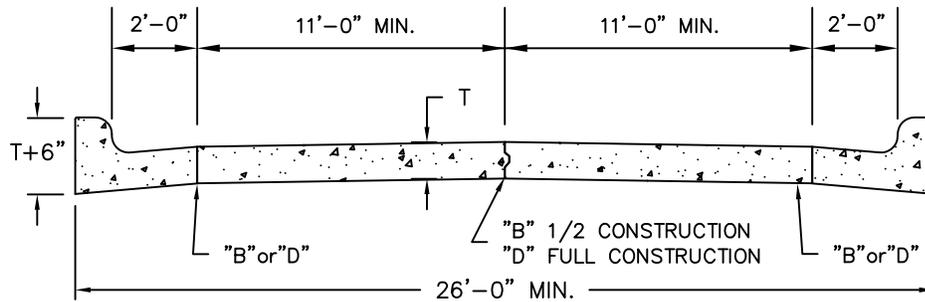


DETAIL KEY

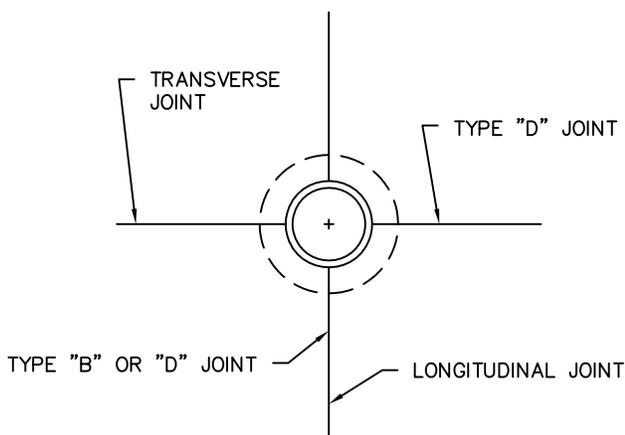
KEYWAY FORMED BY FASTENING KEY TO FORM



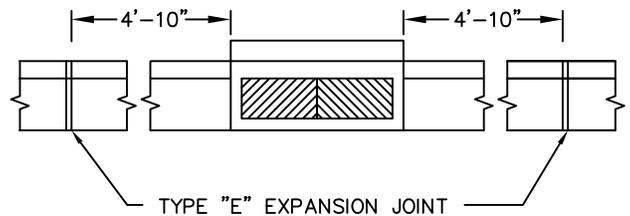
TYPICAL ALLEY SECTION



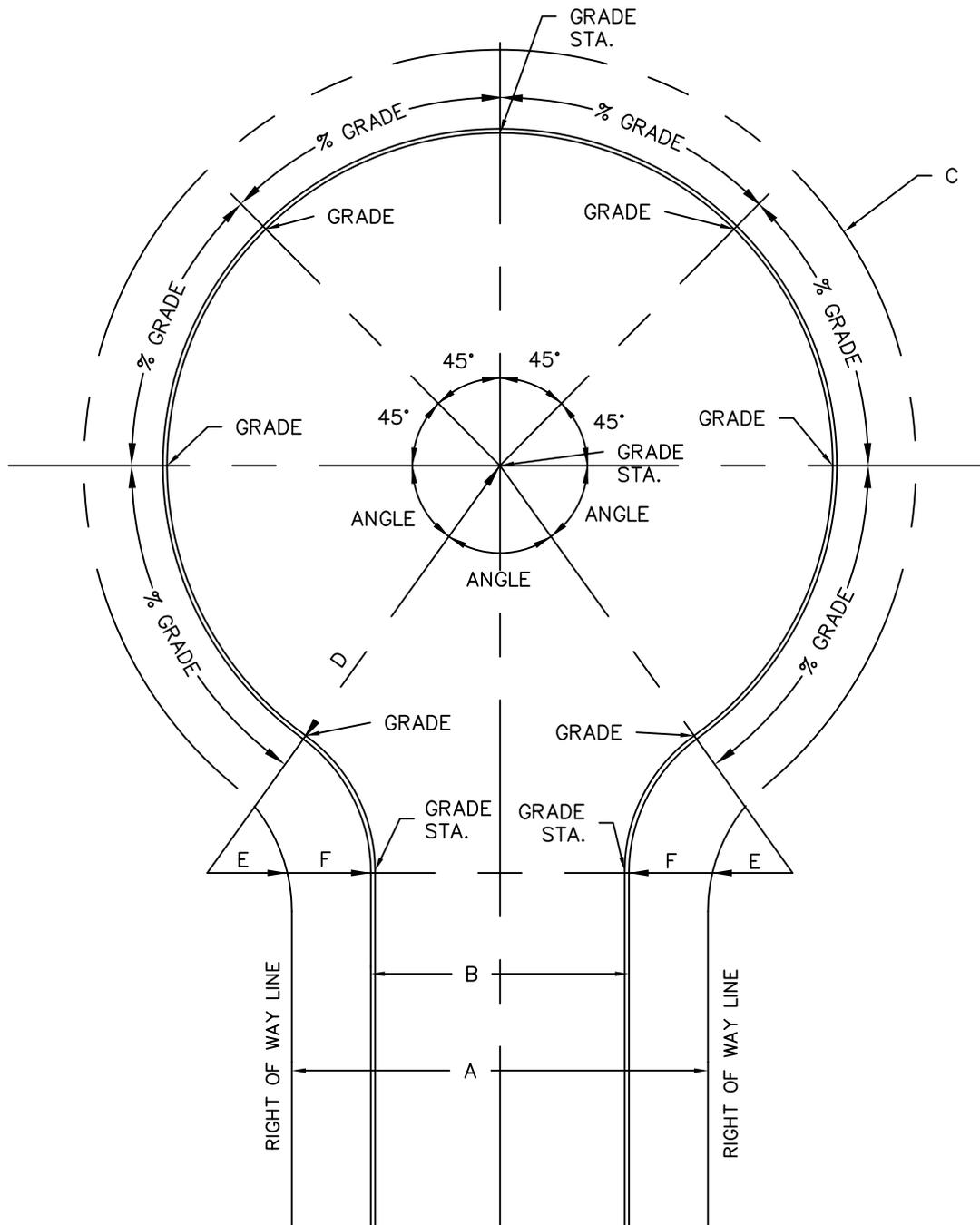
CONCRETE PAVEMENT – JOINT LOCATIONS



MANHOLE JOINT DETAIL



JOINT LOCATION AT INLETS

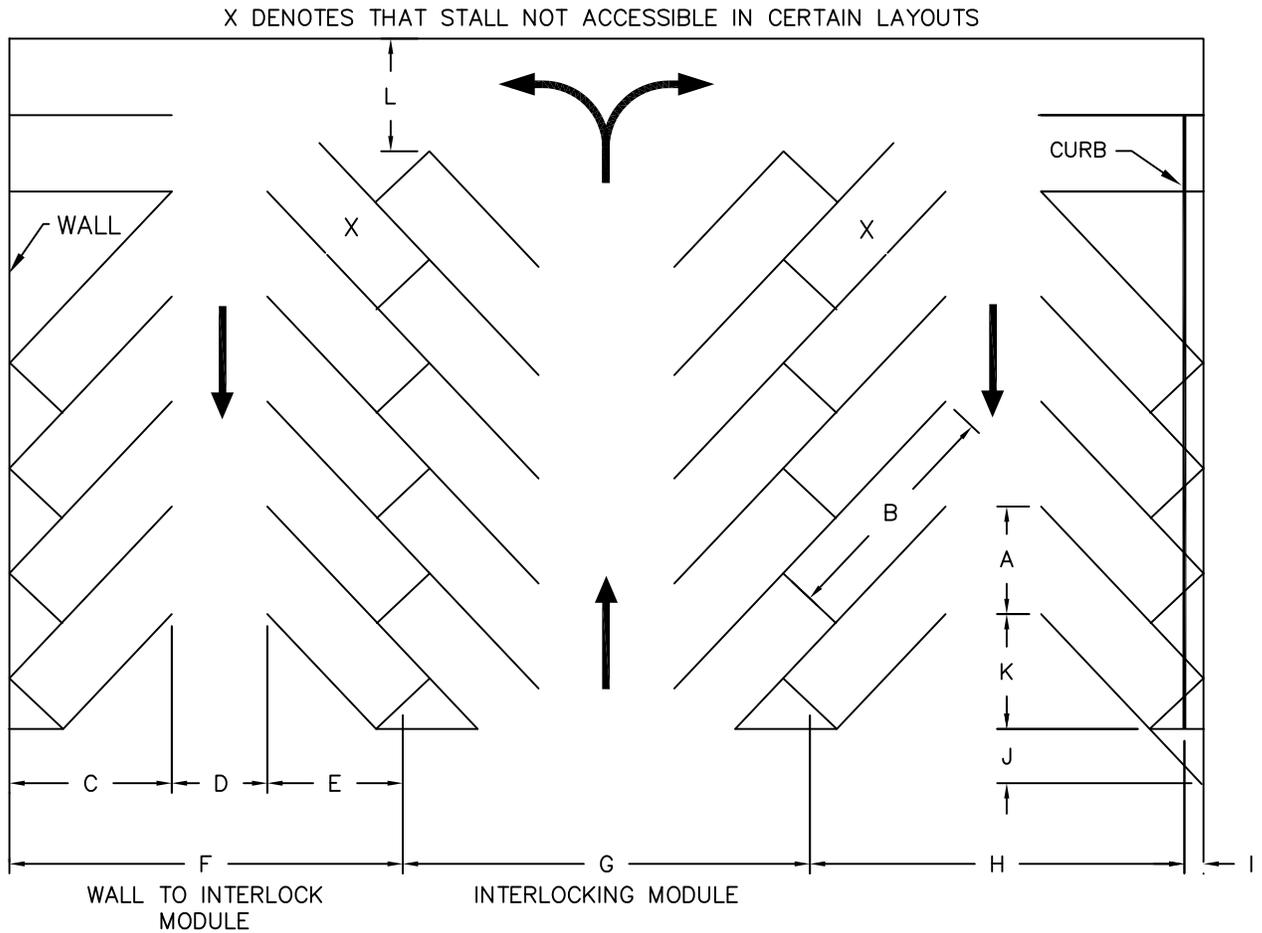


MINIMUM DIMENSIONS (FEET)

DESCRIPTION	KEY	STANDARD	INDUSTRIAL
STREET RIGHT-OF-WAY WIDTH	A	50	60
STREET PAVEMENT WIDTH TO BACK OF CURB	B	27	37
CUL-DE-SAC RIGHT-OF-WAY RADIUS	C	60	75
CUL-DE-SAC PAVEMENT RADIUS TO BACK OF CURB	D	48.5	63.5
INTERNAL CURVE, RIGHT-OF-WAY RADIUS	E	20	20
INTERNAL CURVE, BACK OF CURB RADIUS	F	31.5	31.5

NOTE: MAXIMUM LENGTH OF CUL-DE-SAC SHALL BE 800 FEET.

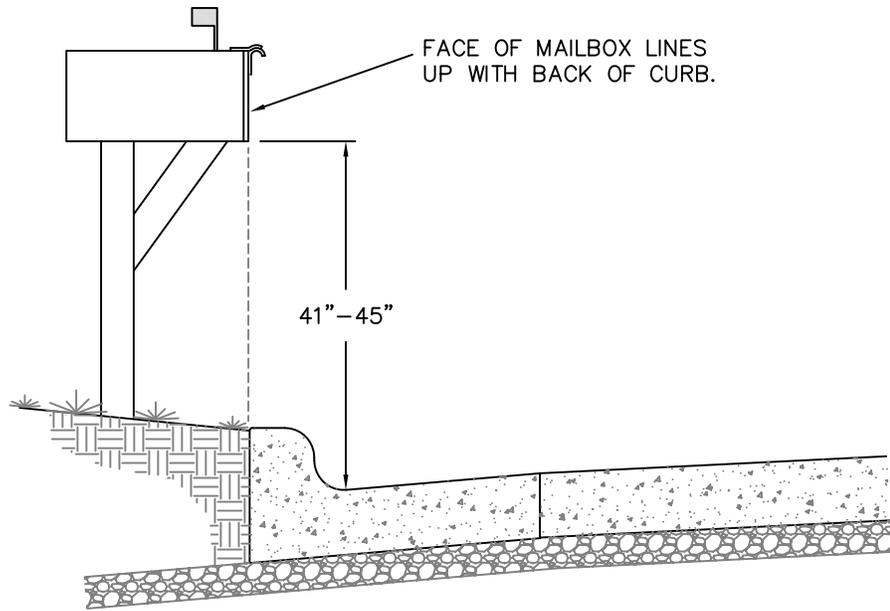
MINIMUM REQUIREMENTS FOR LAYOUT ELEMENTS



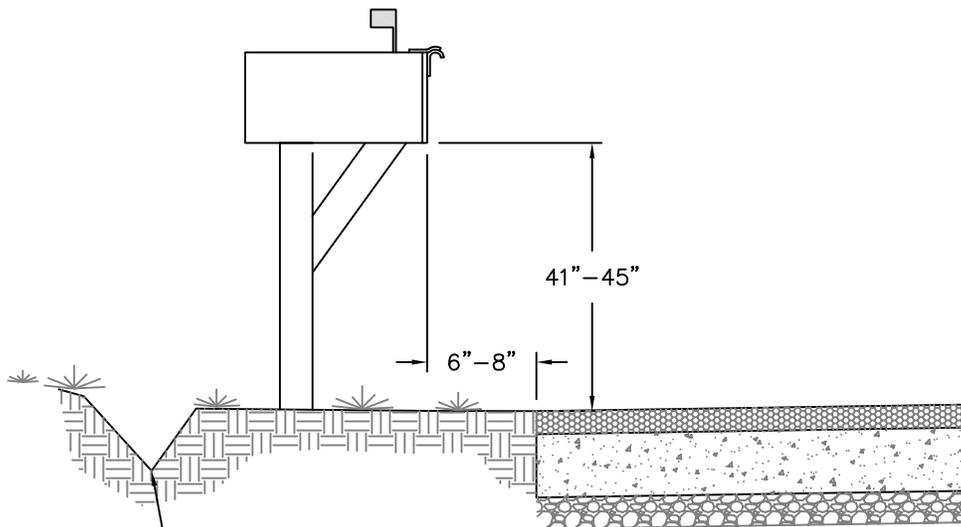
PARKING LAYOUT DIMENSIONS (IN FEET) FOR 9 FT. STALLS AT VARIOUS ANGLES

DIMENSION	SYMBOL	90°	75°	60°	45°	30°
STALL WIDTH, PARALLEL TO AISLE	A	9.0	9.3	10.4	12.7	18.0
STALL LENGTH OF LINE	B	18.5	20.0	22.0	25.0	34.1
STALL DEPTH TO WALL	C	18.5	19.5	19.0	17.5	17.1
AISLE WIDTH BETWEEN STALL LINES	D	26.0	23.0	16.0	12.0	10.0
STALL DEPTH, INTERLOCK	E	18.5	18.8	17.5	15.3	13.2
MODULE, WALL TO INTERLOCK	F	63.0	61.3	52.5	44.8	40.3
MODULE, INTERLOCKING	G	63.0	61.0	51.0	42.6	36.4
MODULE, INTERLOCK TO CURB FACE	H	60.5	58.8	50.2	42.8	38.8
BUMPER OVERHANG (TYPICAL)	I	2.5	2.5	2.3	2.0	1.5
OFFSET	J	0.0	0.5	2.7	6.3	13.5
SETBACK	K	0.0	5.0	8.3	11.0	16.0
CROSS AISLE, ONE-WAY	L	14.0	14.0	14.0	14.0	14.0
CROSS AISLE, TWO-WAY	-	24.0	24.0	24.0	24.0	24.0

NOTE: ANY PARKING LAYOUT OTHER THAN SHOWN MUST BE APPROVED BY THE CITY TRAFFIC ENGINEER.

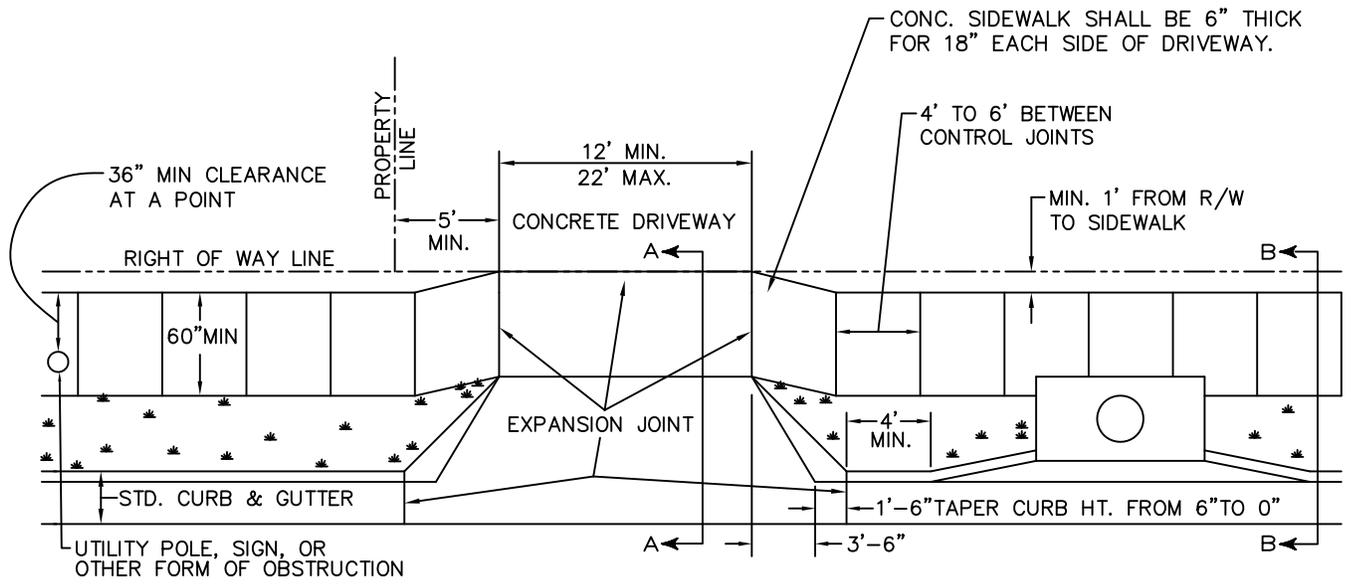


IMPROVED STREETS



UNIMPROVED STREETS

NOTE:
DITCH SECTION BEHIND MAILBOX
WITH A PEDESTRIAN CROSSING.

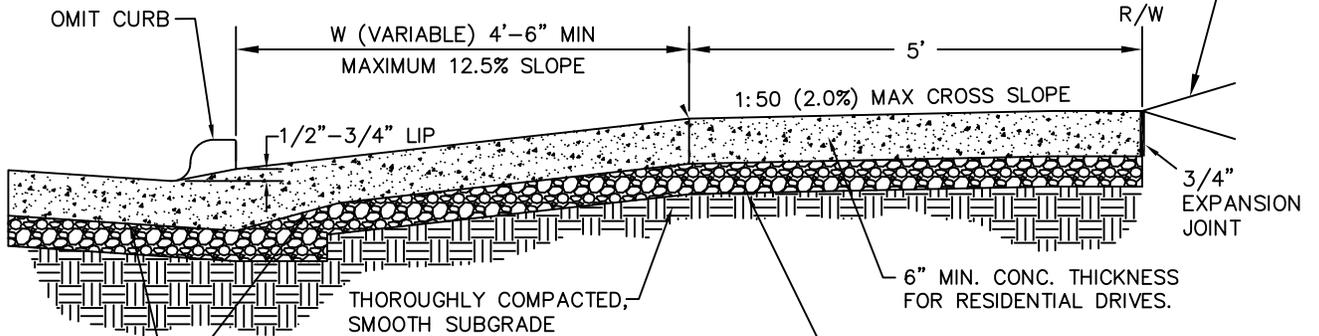


TYPICAL DRIVEWAY PLAN VIEW

NOTE:

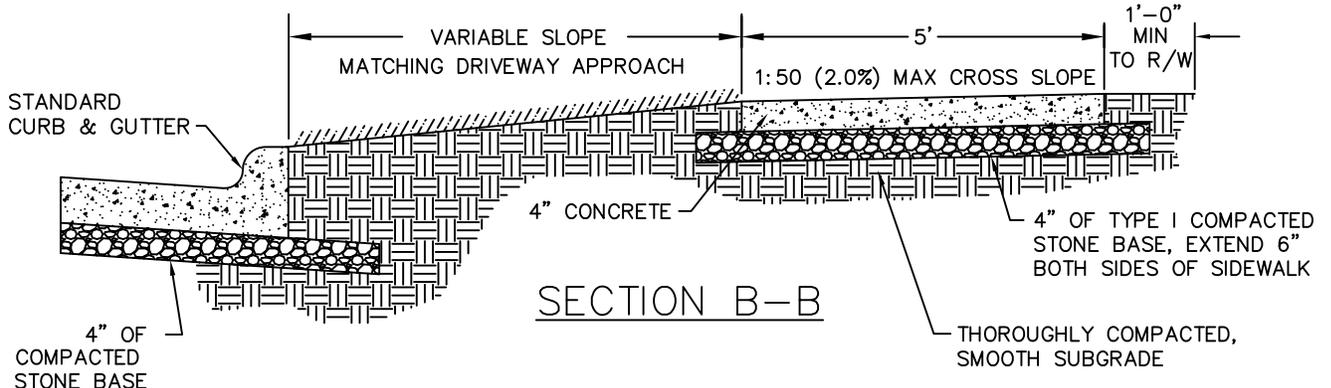
ANY PART OF AN ACCESSIBLE ROUTE WITH A RUNNING SLOPE GREATER THAN 1:20 SHALL BE CONSIDERED A RAMP AND SHALL COMPLY WITH THOSE REGULATIONS.

SLOPE VARIES -3% TO 12% WITHIN 10' OF RIGHT OF WAY. THE ELEVATION AT THE RIGHT OF WAY LINE SHALL BE A MINIMUM OF 6" ABOVE THE FLOWLINE OF THE GUTTER

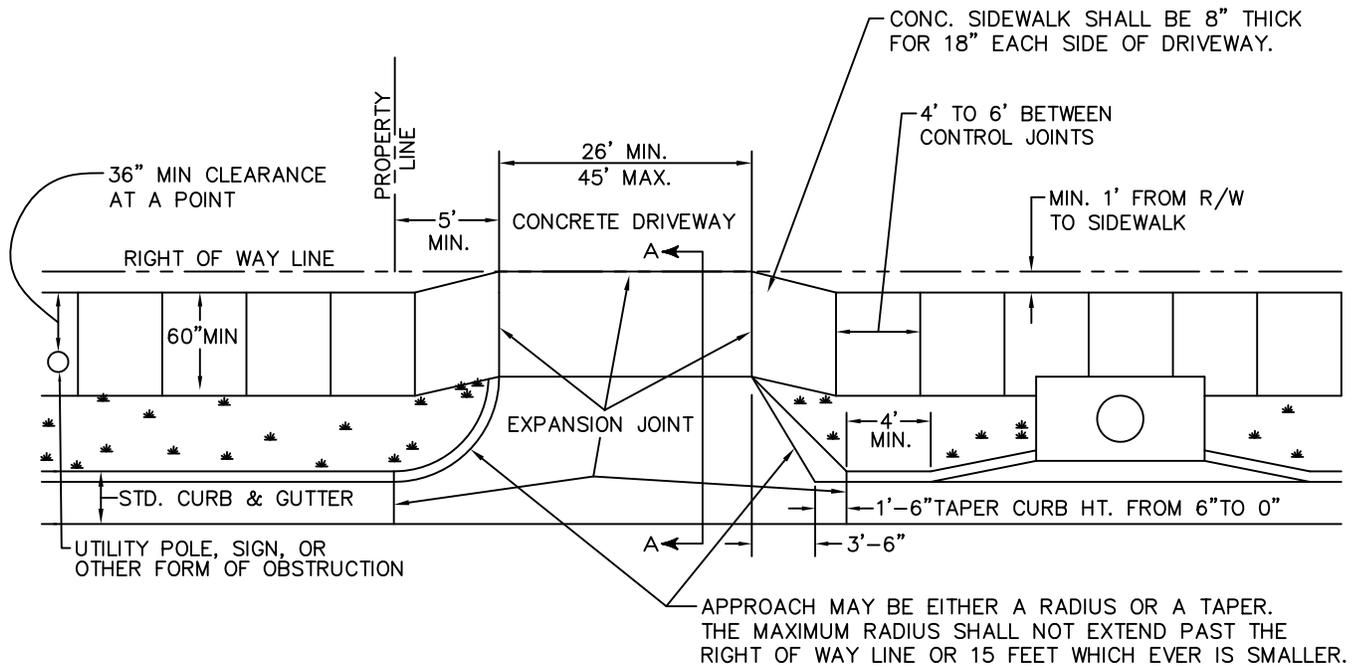


SECTION A-A

NOTE:
GUTTER SECTION AND DRIVEWAY TO BE BE POURED MONOLITHIC.



SECTION B-B

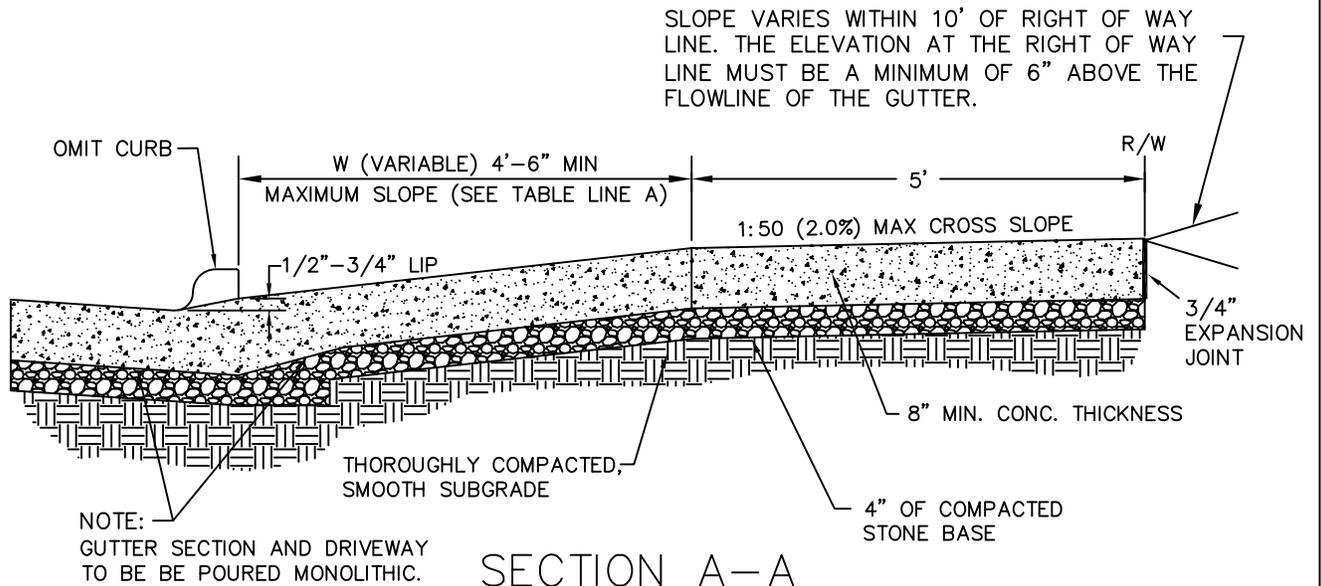


TYPICAL DRIVEWAY PLAN VIEW

NOTE:

ANY PART OF AN ACCESSIBLE ROUTE WITH A RUNNING SLOPE GREATER THAN 1:20 SHALL BE CONSIDERED A RAMP AND SHALL COMPLY WITH THOSE REGULATIONS.

	REQUIRED DRIVEWAY GRADES			
	MAJOR ARTERIAL	SECONDARY ARTERIAL	COLLECTOR	NON-RESIDENTIAL LOCAL
A. DRIVEWAY APPROACH GRADE	1/4in/ft to 1/2in/ft	1/4in/ft to 5/8in/ft	1/4in/ft to 3/4in/ft	1/4in/ft to 1in/ft
B. MAXIMUM CHANGE OF GRADE AT BACK OF SIDEWALK	4%	5%	6%	8%
C. SLOPE WITHIN 10 FEET OF RIGHT-OF-WAY LINE	-2% to 6% 1/4in/ft to 3/4in/ft	-3% to 7% -3/8in/ft to 7/8in/ft	-4% to 8% -1/2in/ft to 1in/ft	-6% to 10% -3/4in/ft to 1-1/4in/ft



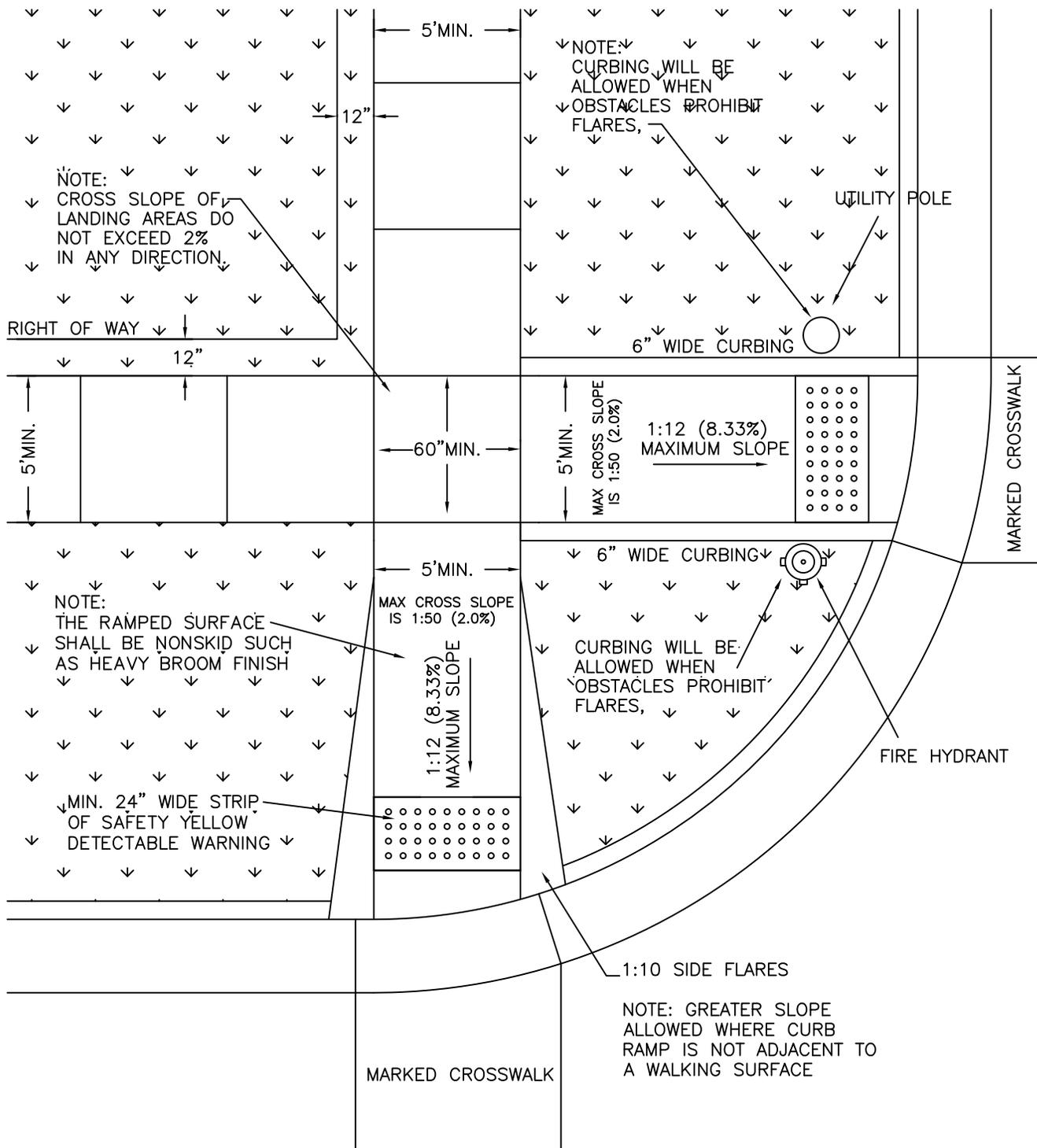
REVISED: 10-17-05; REMOVED REINFORCEMENT REQUIREMENTS.

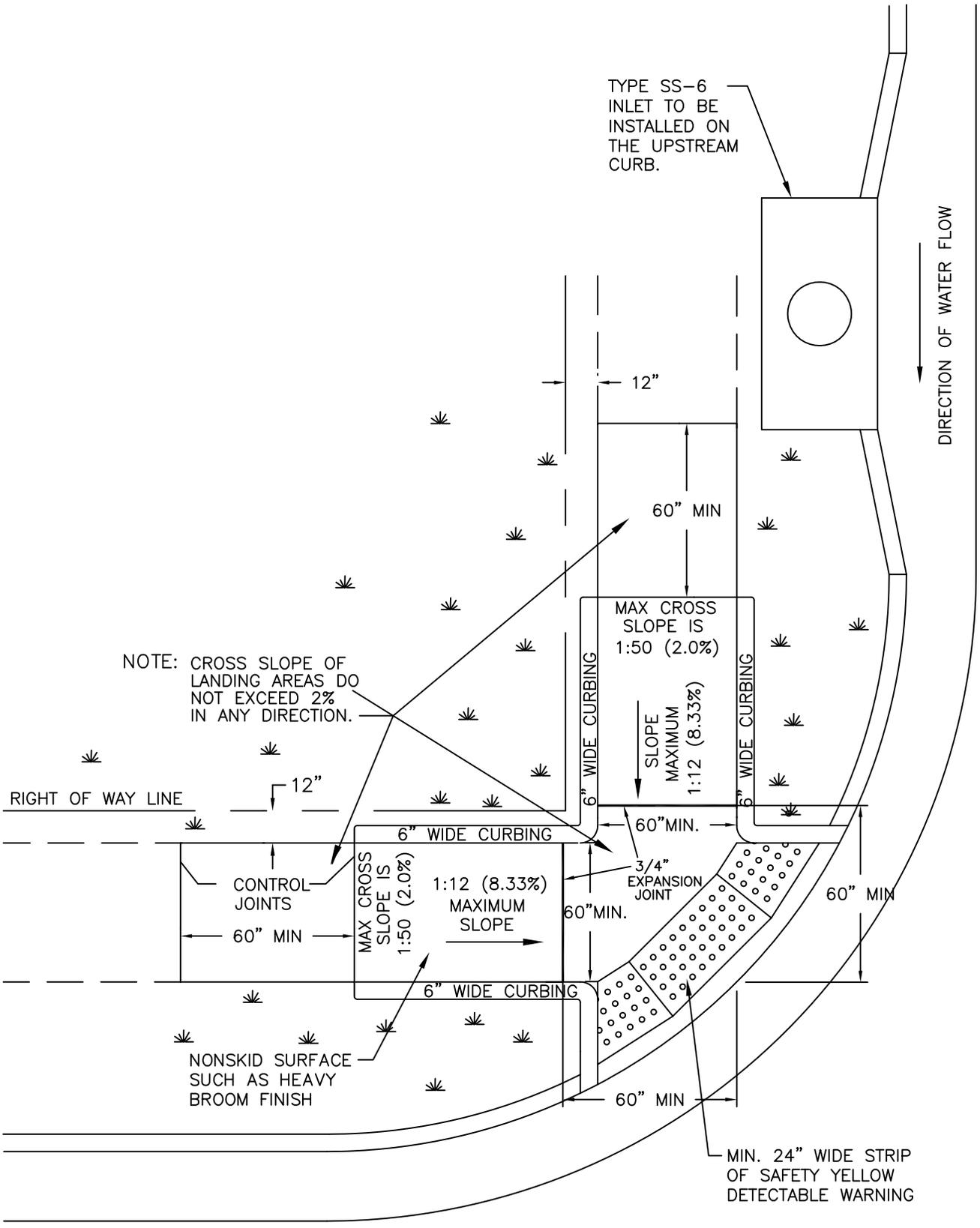
DEPARTMENT OF PUBLIC WORKS
SPRINGFIELD, MO.

TYPICAL COMMERCIAL
DRIVEWAY & SIDEWALK

REVISED: 7/1/2013

ST-9





NOTE: CROSS SLOPE OF LANDING AREAS DO NOT EXCEED 2% IN ANY DIRECTION.

RIGHT OF WAY LINE

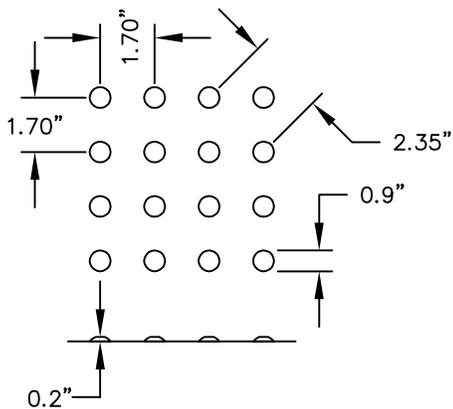
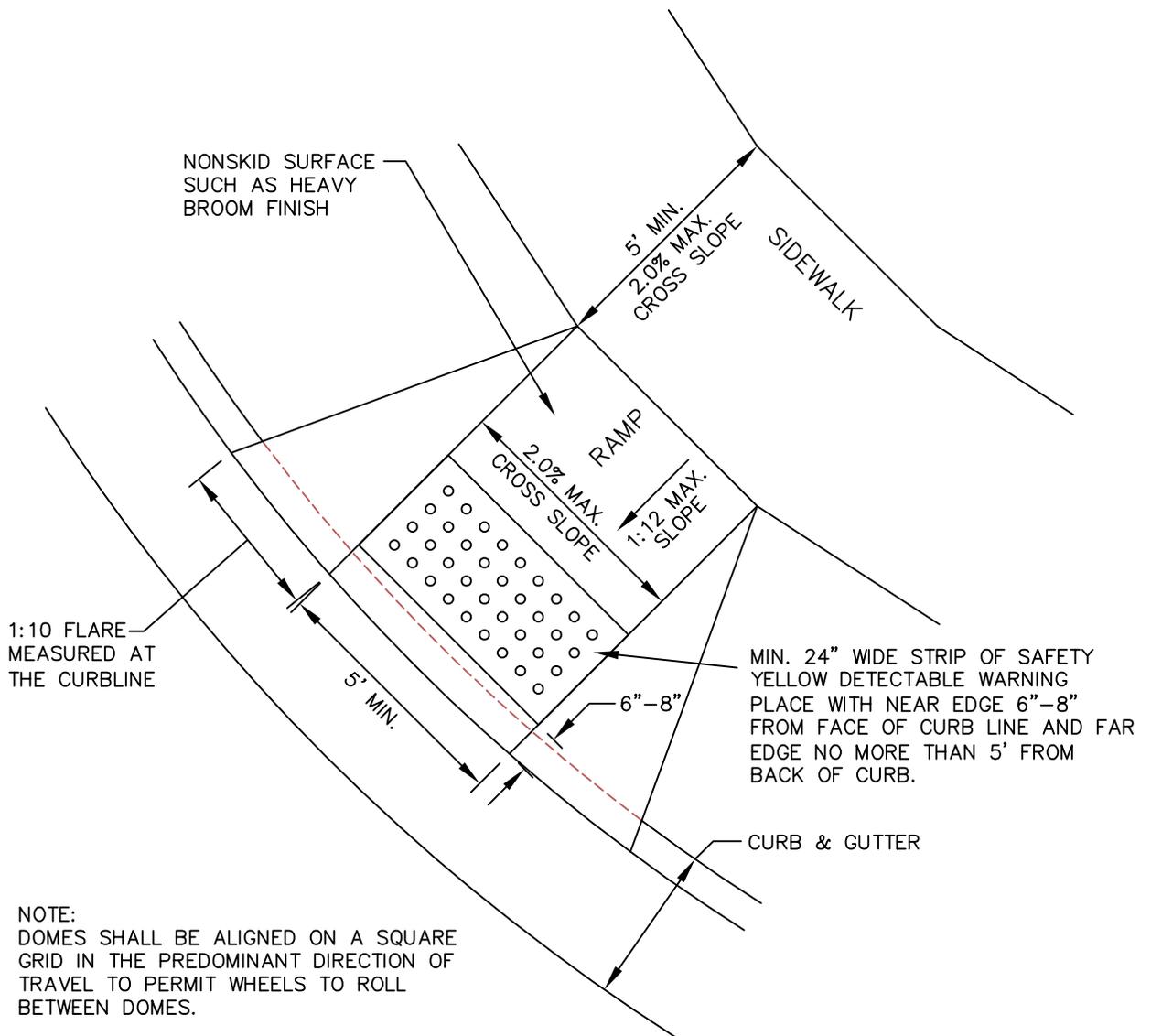
CONTROL JOINTS

NONSKID SURFACE SUCH AS HEAVY BROOM FINISH

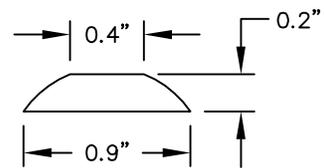
TYPE SS-6 INLET TO BE INSTALLED ON THE UPSTREAM CURB.

DIRECTION OF WATER FLOW

NOTE: USE CURB RAMP STYLE II WHEN DISTANCE FROM FACE OF CURB AND THE RIGHT OF WAY LINE IS LESS THAN AN ALLOWABLE DISTANCE TO INSTALL A CURB RAMP TYPE I WITH A 1:12 (8.33%) RUNNING SLOPE.



DOMES SPACING



DOMES SECTION

SAW CUT EDGE AFTER
EXCAVATION AND BACKFILL
ARE COMPLETED

8" CONCRETE

1"x18" DOWEL BAR
AT 24" O.C.

EXISTING PAVEMENT

12" MIN.

UNDISTURBED
EARTH

KEEP TRENCH WIDTH AS
NARROW AS POSSIBLE

THOROUGHLY COMPACTED
GRANULAR BACKFILL

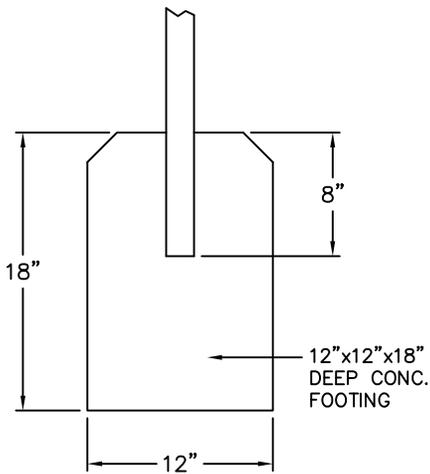
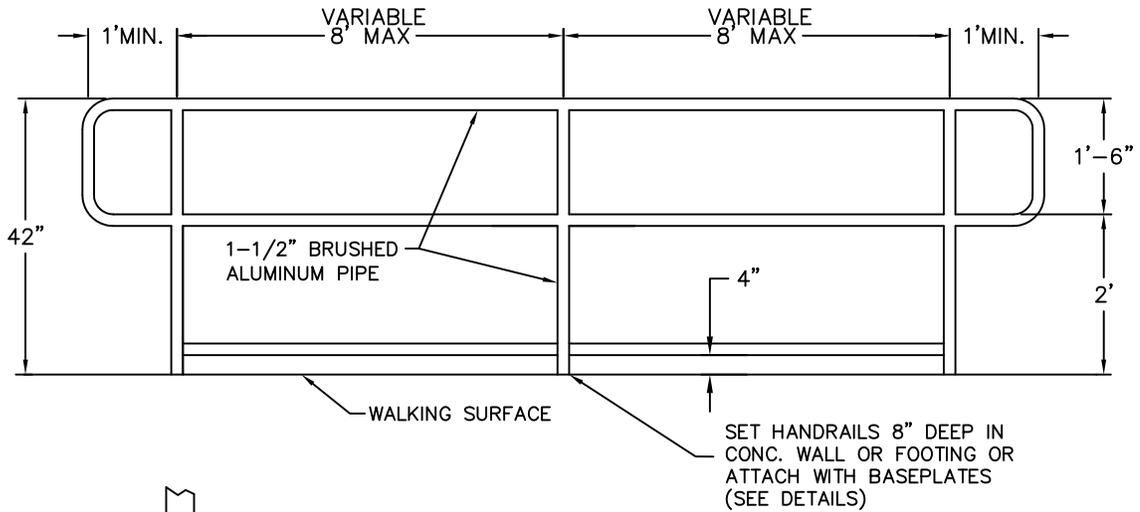
PIPE

SAW CUT FOR
EXCAVATION

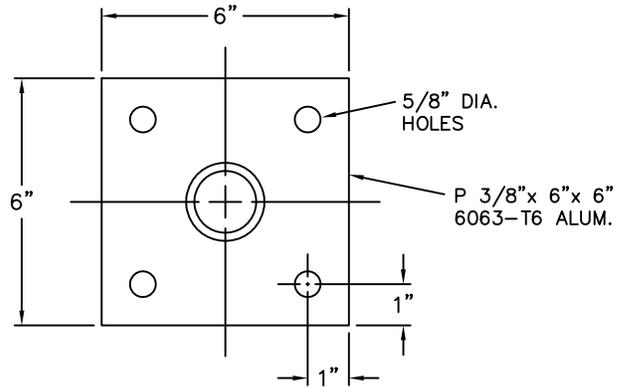
NOTES:

*OMIT DOWEL BARS WHEN REPAIRING ASPHALT PAVEMENT

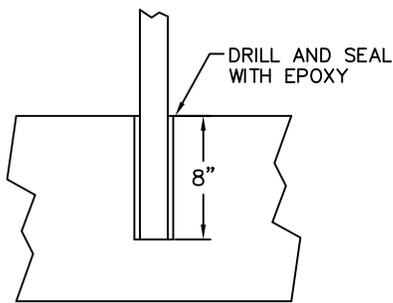
*REPAIR SHALL CONFORM TO ALL CONSTRUCTION JOINT REQUIREMENTS



CONC. FOOTING DETAIL



BASEPLATE DETAIL



HEADWALL DETAIL

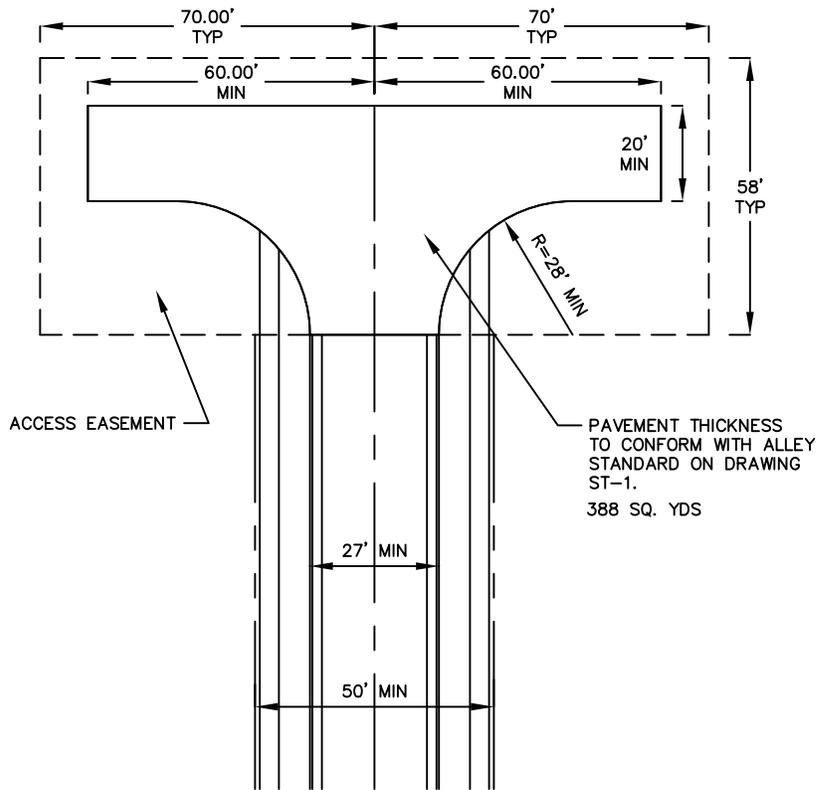
RAILING & POST SPECIFICATIONS		
TYPE	SIZE (DIA.)	WEIGHT (LBS./FT.)
ROUND	1-1/2"	ALUMINUM 0.940

GENERAL NOTES:

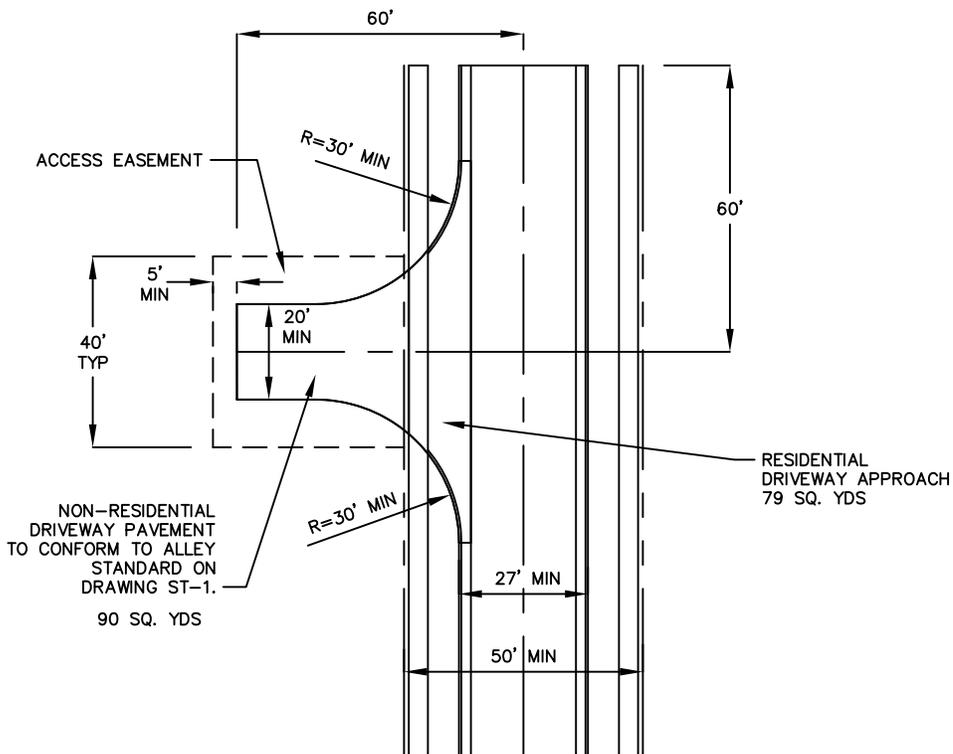
RAILINGS, POSTS, AND BASEPLATES SHALL BE ALUMINUM ALLOY 6061-T6 OR 6063-T6.

IF PRE-MANUFACTURED HANDRAIL COMPONENTS ARE TO BE USED, PRIOR APPROVAL IS REQUIRED.

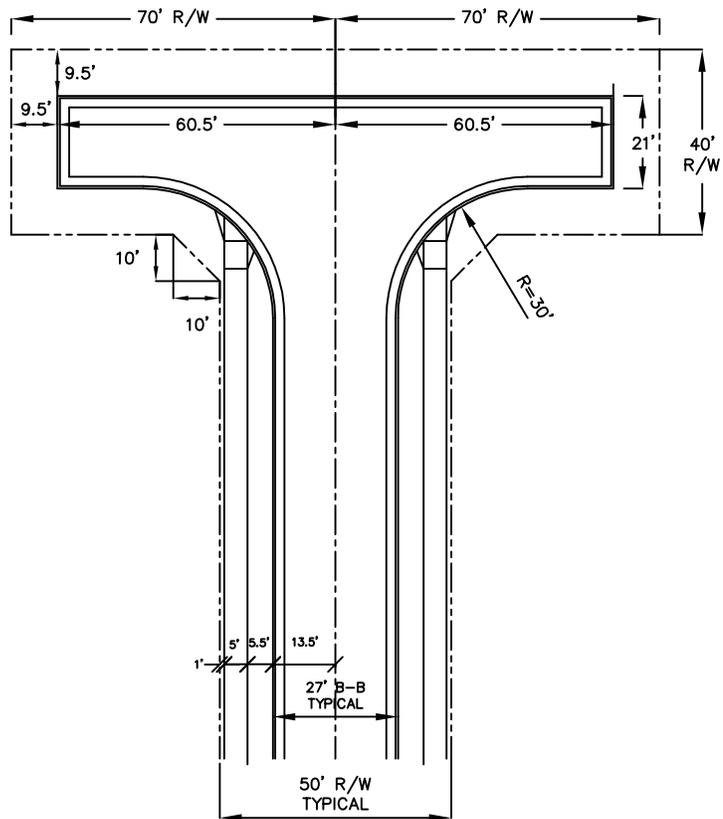
ALL JOINTS SHALL BE CONTINUOUS WELDED.



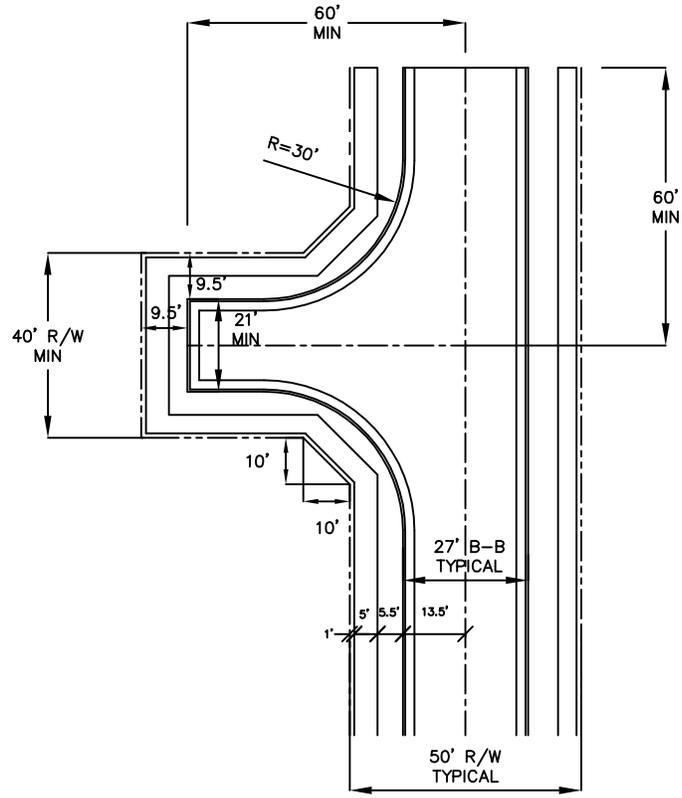
"T" TYPE



"BRANCH" TYPE



" T " TYPE



" BRANCH " TYPE